



# New Light Vehicles CO<sub>2</sub> Performance in 2011

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14 March 2012

Cars of Tomorrow Conference, Melbourne



# About the National Transport Commission

## **Our vision**

Australia's prosperity and community liveability is enhanced by the movement of people and goods

## **Our mission**

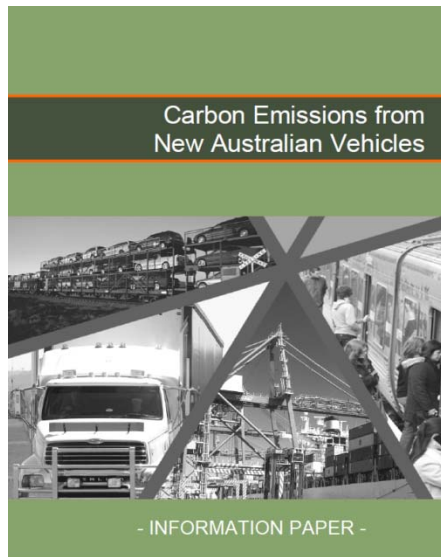
To champion and facilitate changes that improve productivity, safety and environmental outcomes

## **Our role**

By developing national regulatory and operational reform and implementation strategies for road, rail and intermodal transport

# Reports

2005-2009



November 2009



2010



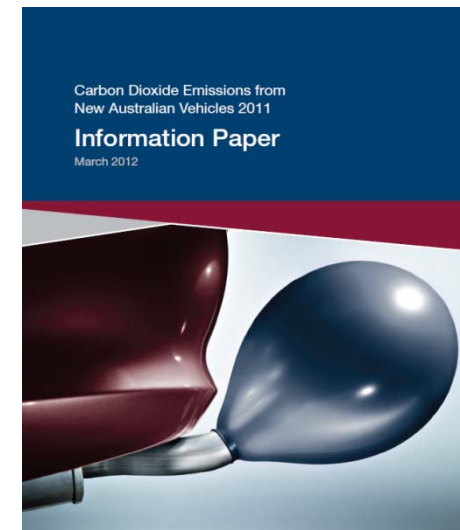
Carbon Dioxide Emissions from  
New Australian Vehicles 2010

**Information Paper**

May 2011



2011



Carbon Dioxide Emissions from  
New Australian Vehicles 2011

**Information Paper**

March 2012

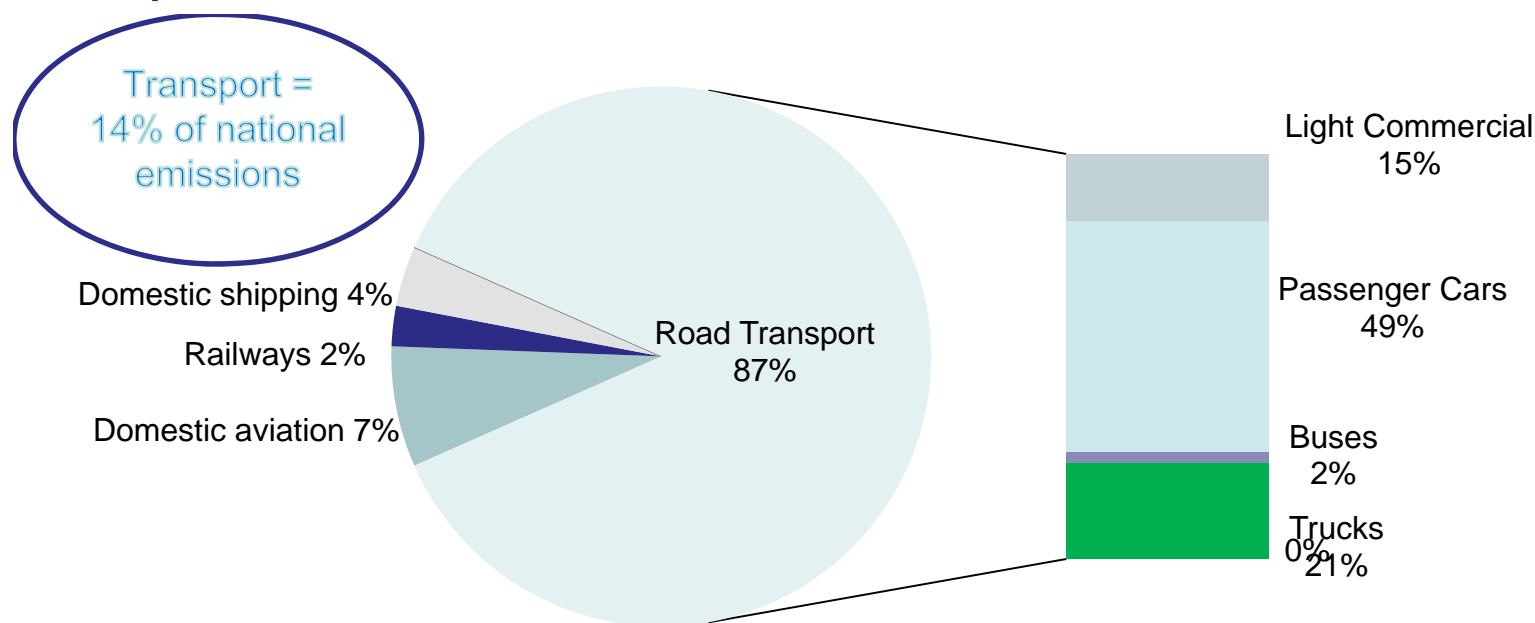


These reports would not be possible without the help and support of the Federal Chamber of Automotive Industries



# Light vehicles a major contributor

## Transport GHG Emissions, Australia 2009

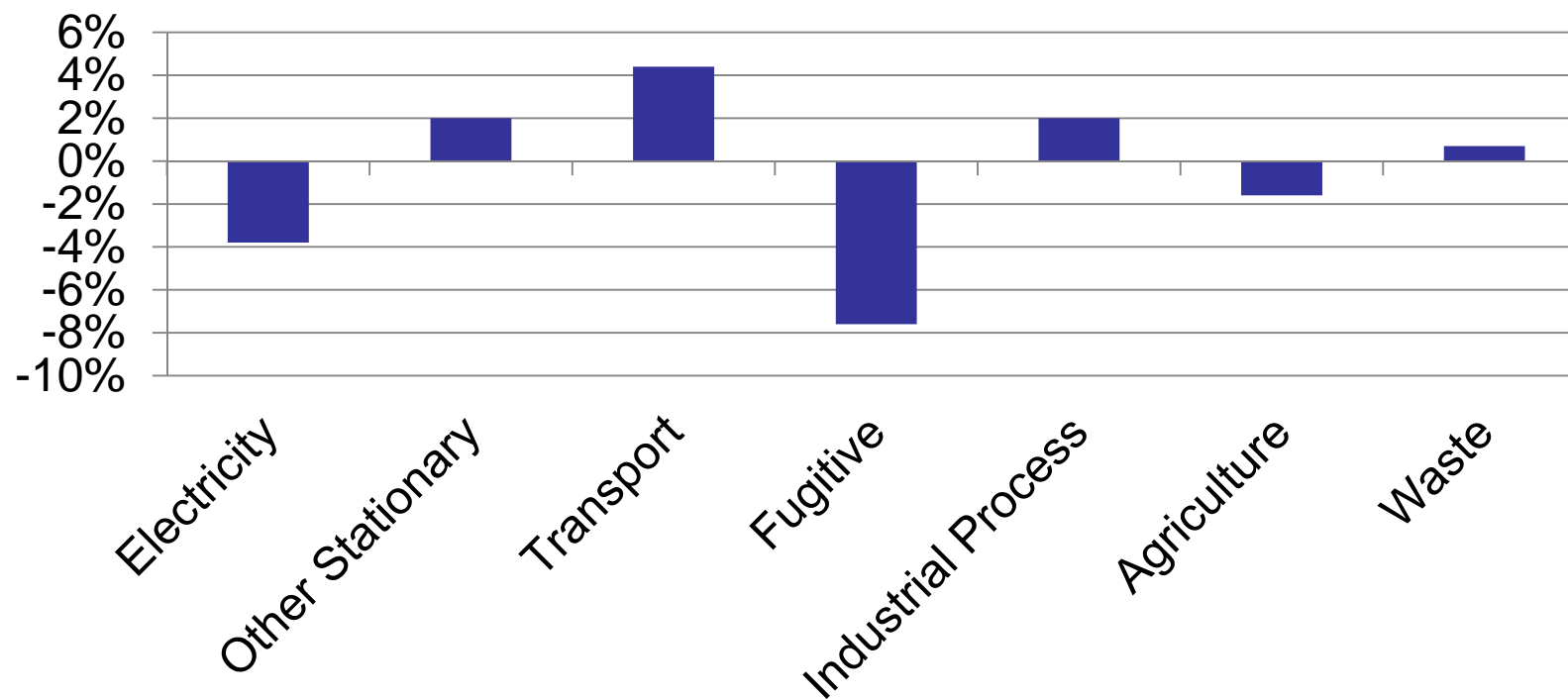


Source: Australia's Emissions Projections, 2010, Department of Climate Change & Energy Efficiency

Note: Does not include international bunkers

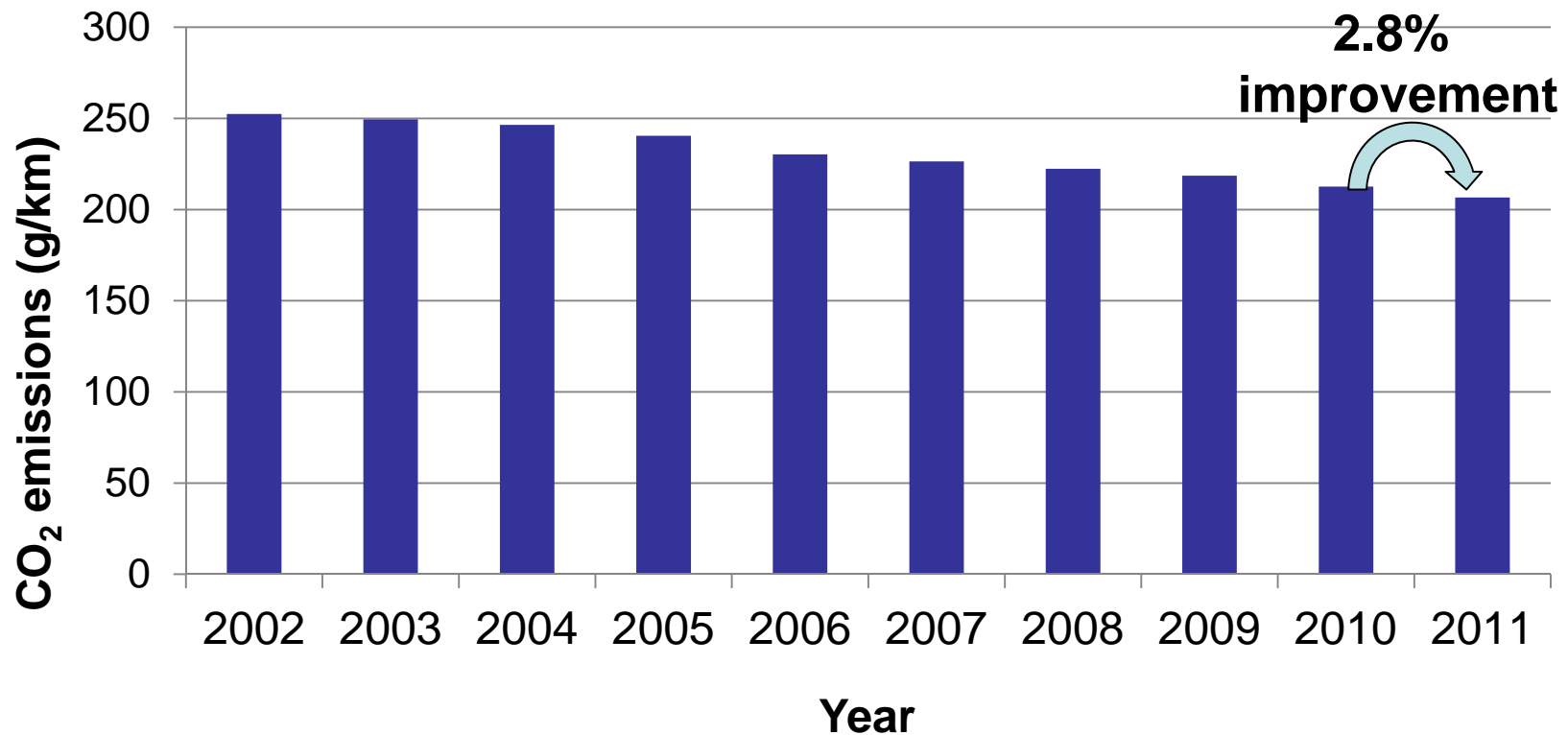
# Recent trends put focus on transport

## Emissions change by source, year to Sept 2011



Source: Australian National Greenhouse Accounts, Sept Qtr 2011, Dept of Climate Change & Energy Efficiency.

# New vehicle CO<sub>2</sub> emissions



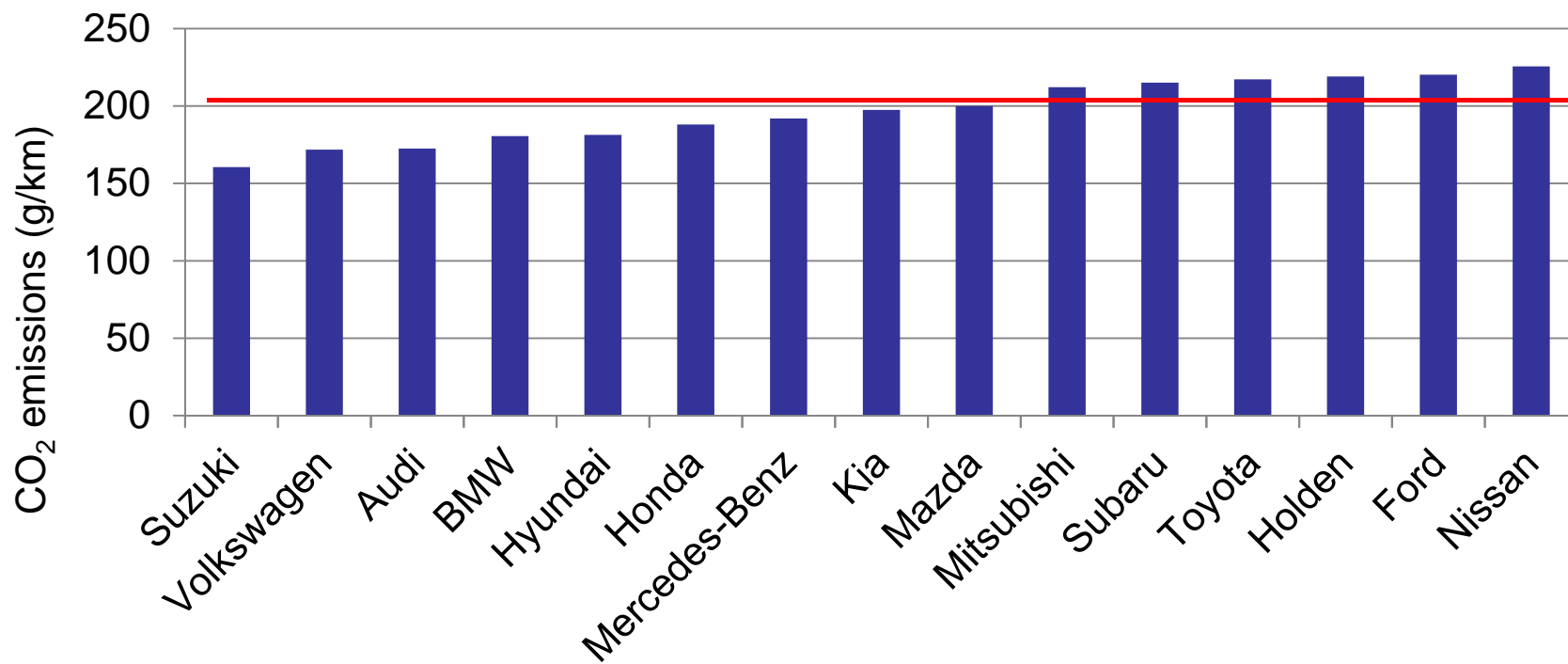


## Passenger vehicles and LCVs

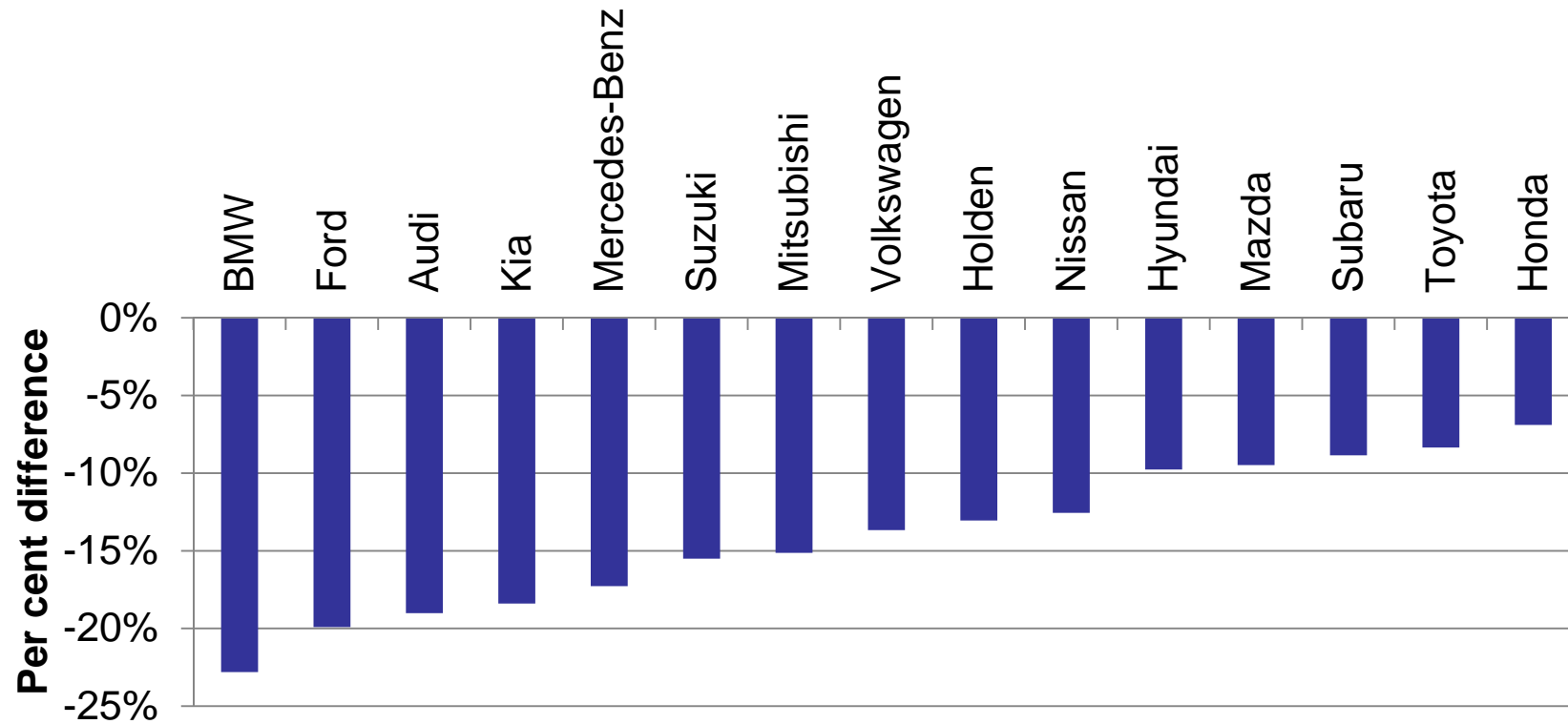
Vehicles	Average CO <sub>2</sub> emissions (g/km)		Change 2005-2011
	2005	2011	
Passenger	231	198	-14.3%
Light commercial	273	245	-10.3%



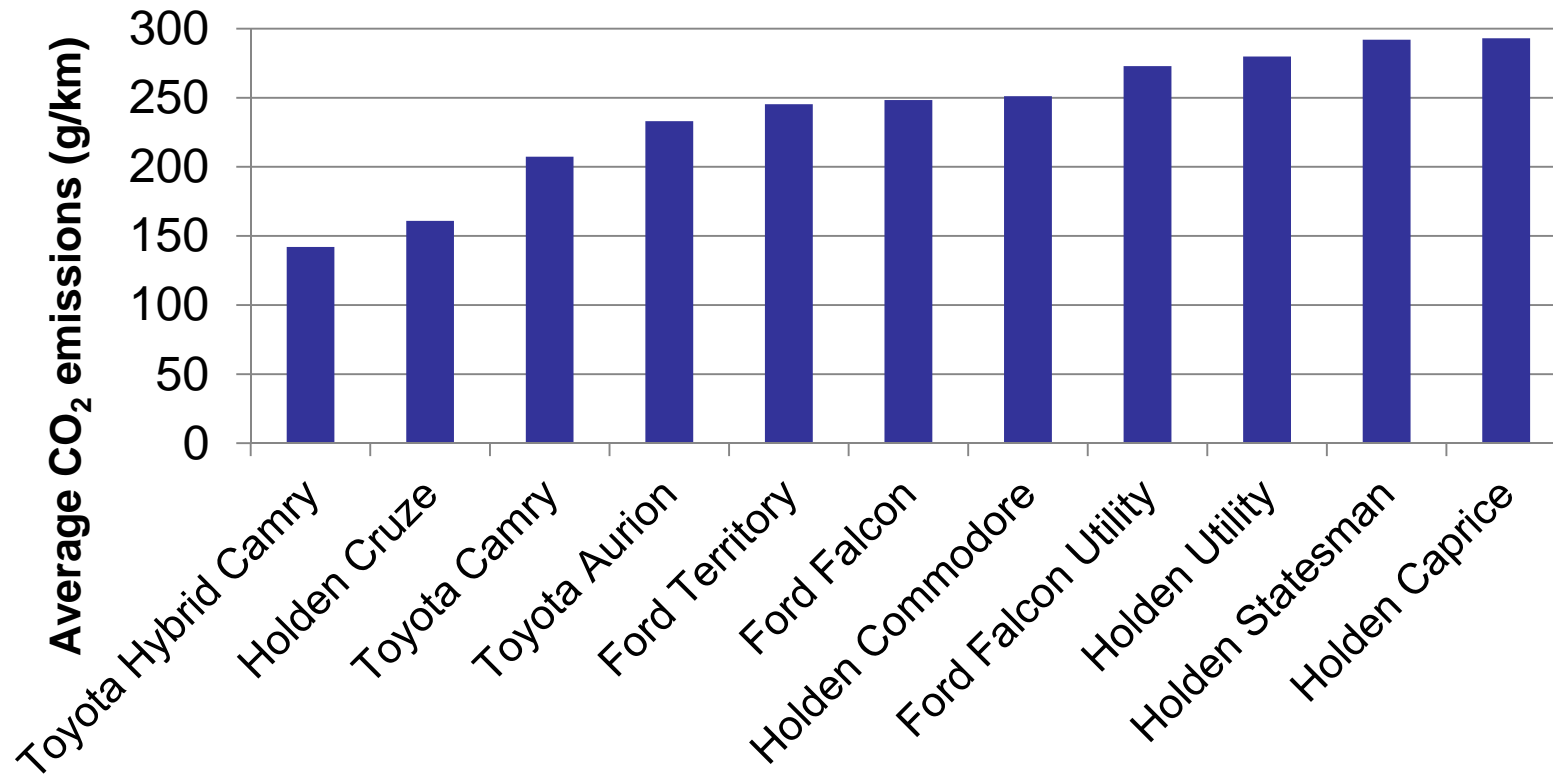
# Suzuki - best performing volume make in 2011



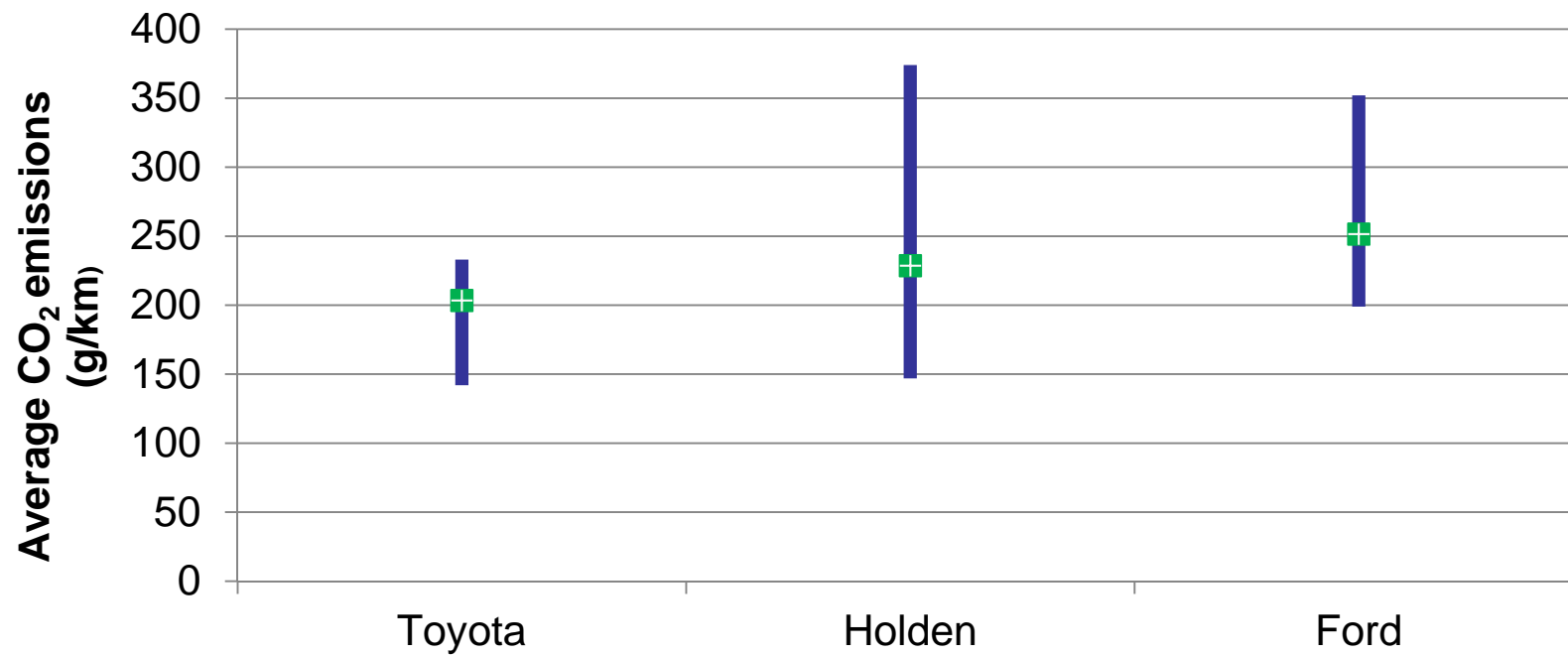
# Make: reductions between 2005-2011



# Australian-made models 2011

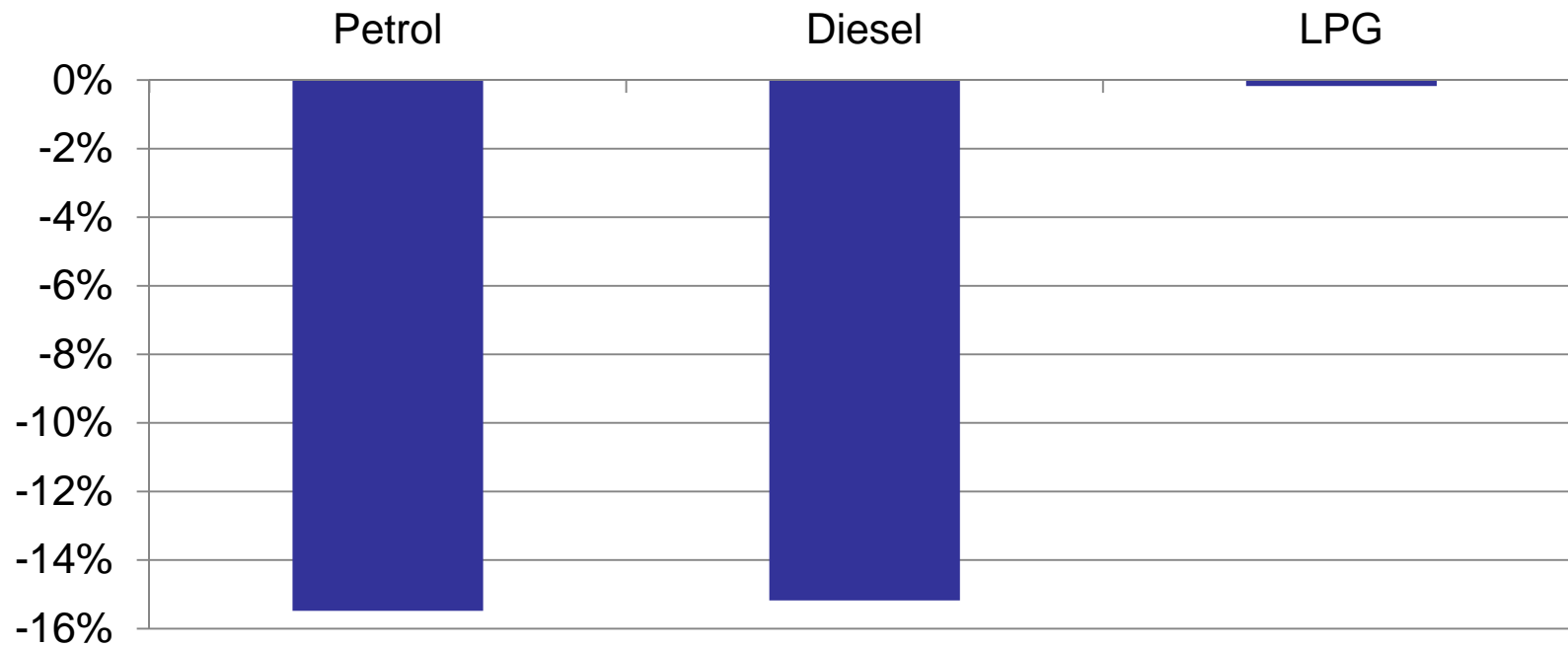


# Australian-made cars 2011



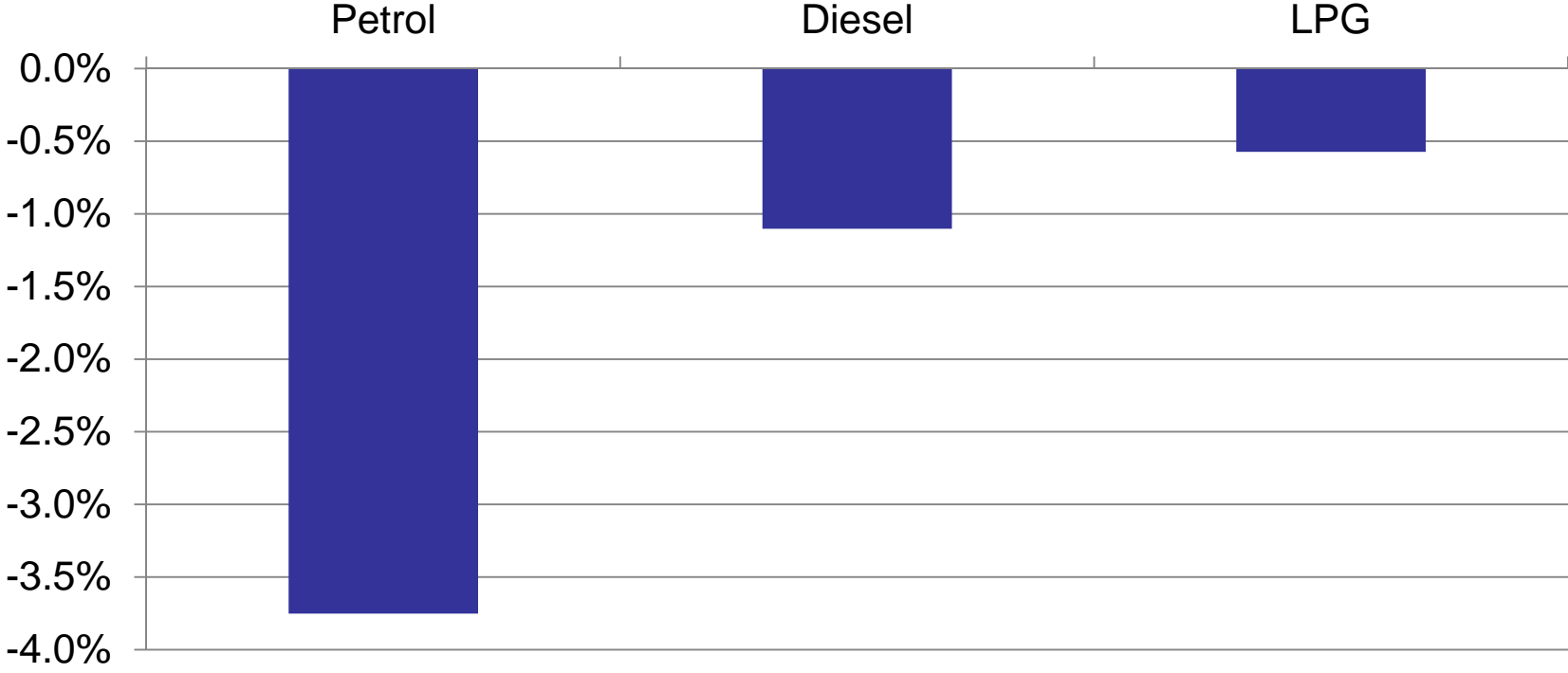


# Fuel type: reduction 2005-2011



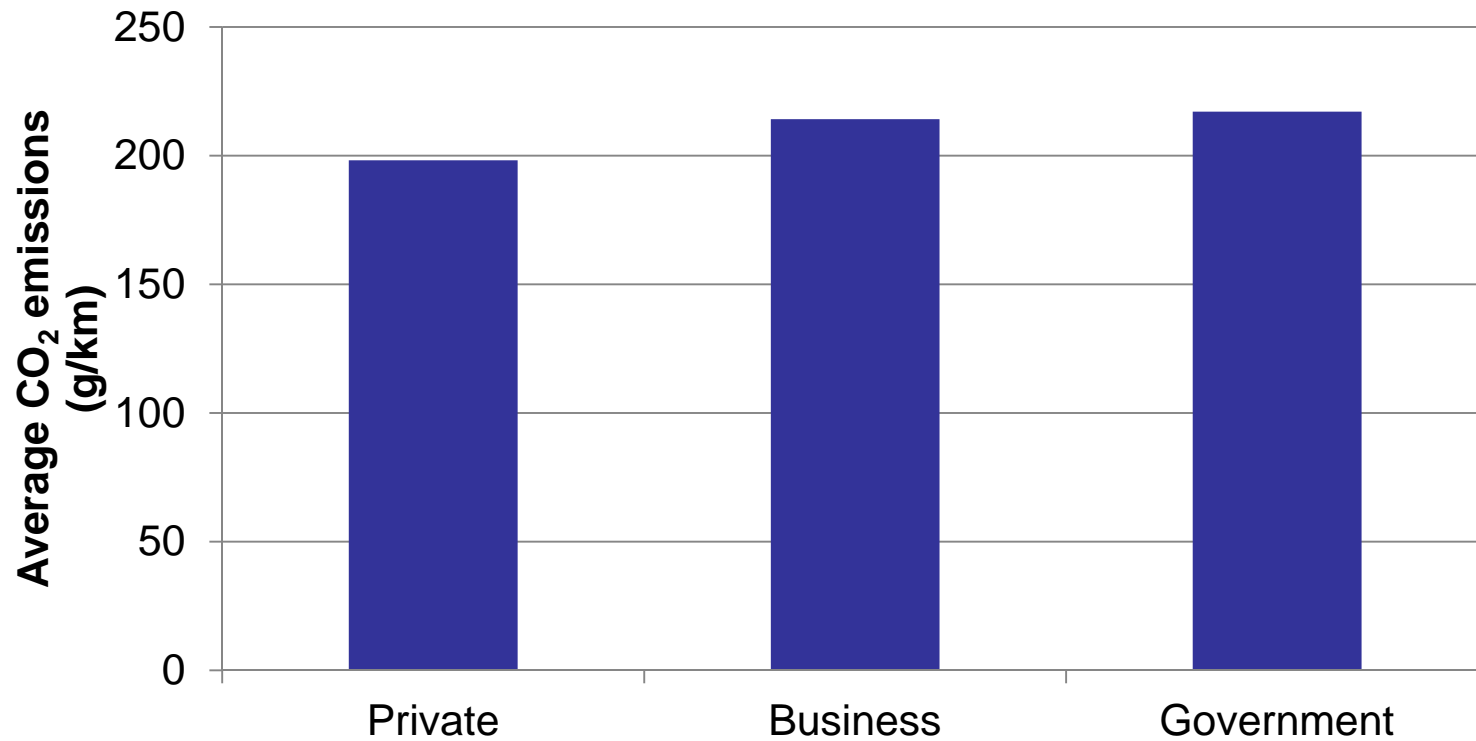


# Fuel type: reduction 2010-2011

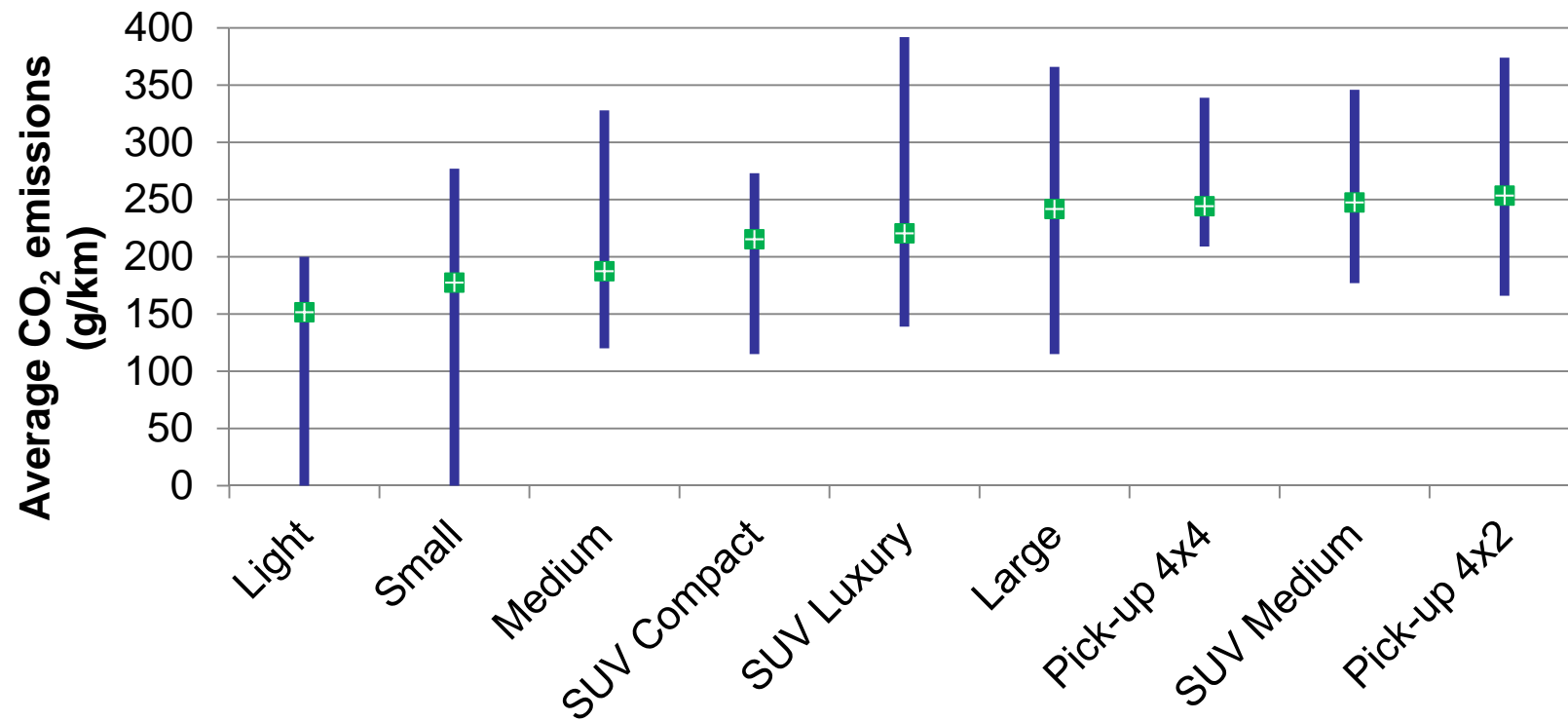




# Buyer type 2011



# Major segments 2011



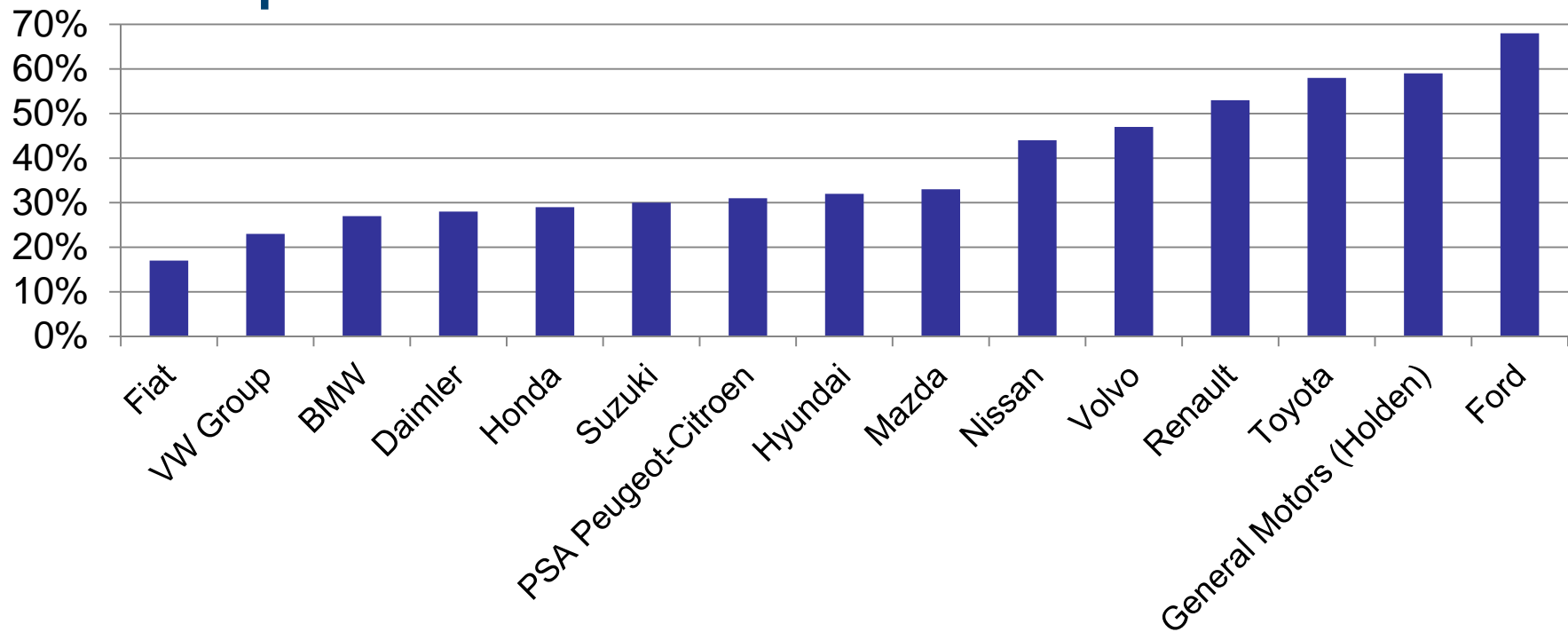




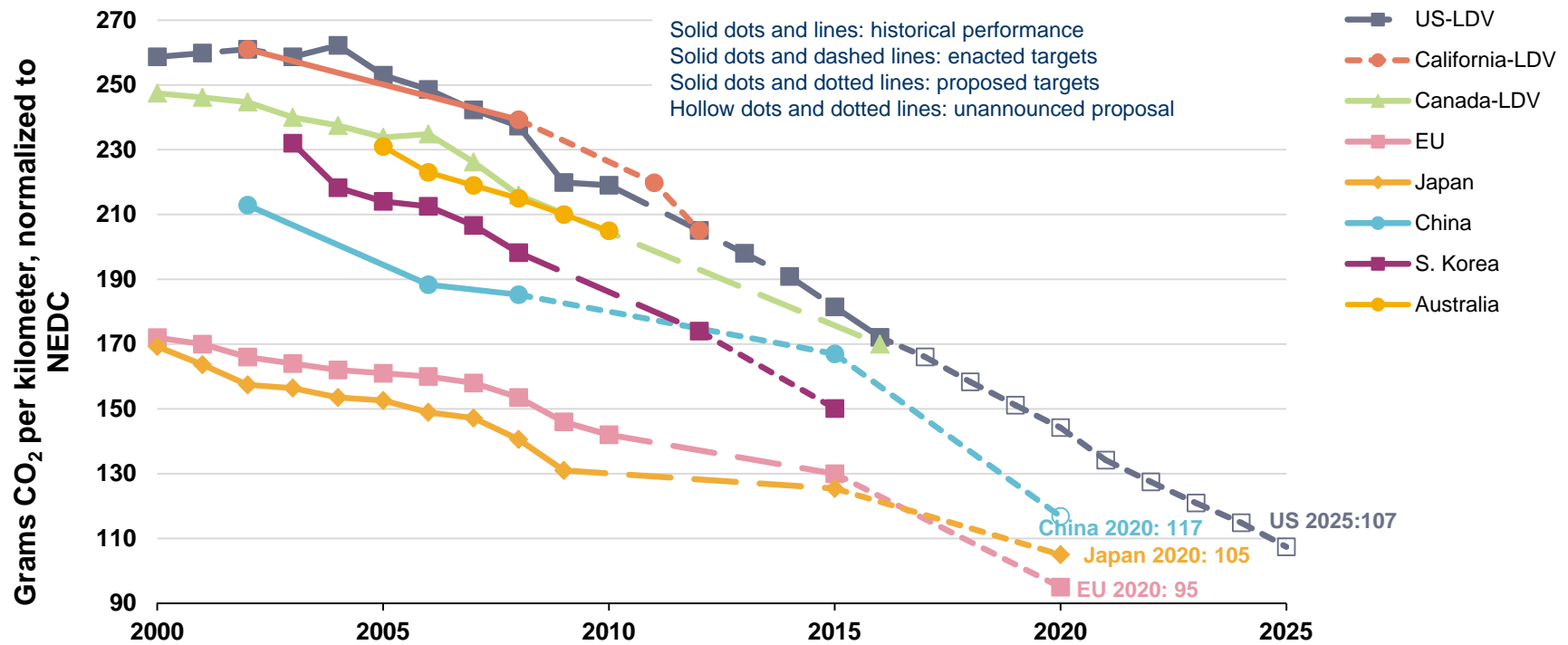
## Low emission vehicles available

- If Australians purchased vehicles with best-in-class emissions, average carbon emissions would be 38% lower in 2011
- 23 models of “green cars” (<120 g/km) available in 2011
- New analysis for 2011 shows that “green cars” or best-in-class vehicles are not amongst the besting selling vehicles

# Higher CO<sub>2</sub> for Australian cars compared to EU in 2010



# Historical fleet CO<sub>2</sub> emissions performance and current or proposed standards (ICCT)



[1] China's target reflects gasoline fleet scenario. If including other fuel types, the target will be lower.

[2] US and Canada light-duty vehicles include light-commercial vehicles.

## 2050: ten fold improvement needed

Year	Average new vehicle emissions
2011	206.6 g/km
2050	20 – 26 g/km



Emissions equivalent to a car running on one tank of petrol per year



# References

ICCT and ClimateWorks Foundation 2011, *The regulatory engine: How smart policy drives vehicle innovation*, January, Washington.

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